



PATUXENT BRANCH TRAIL

DESIGN

Public Input Meeting July 2022

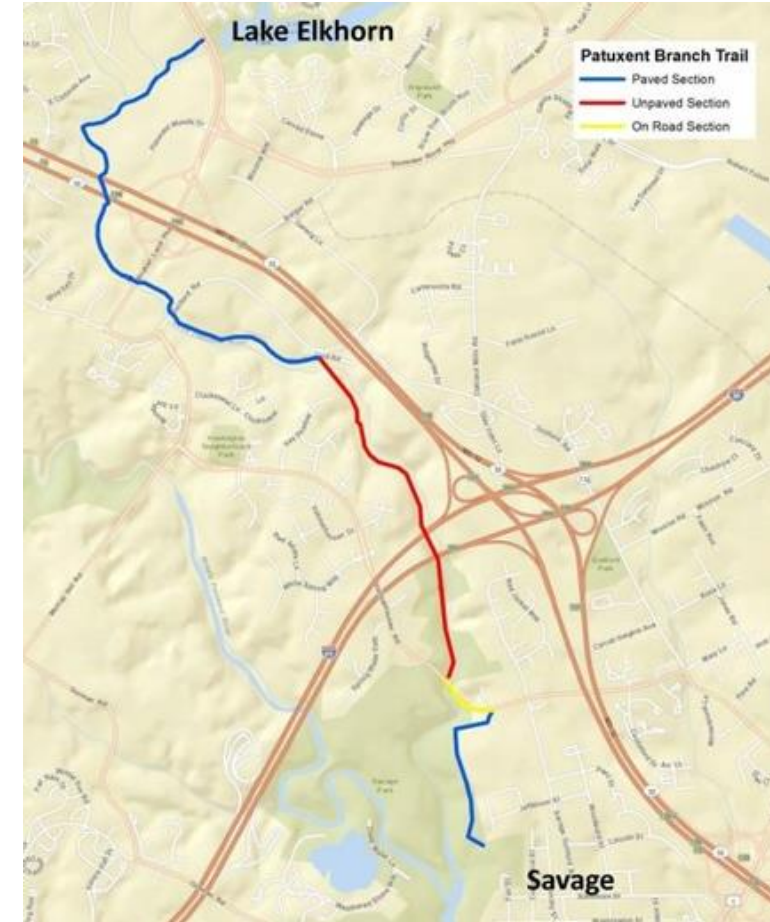
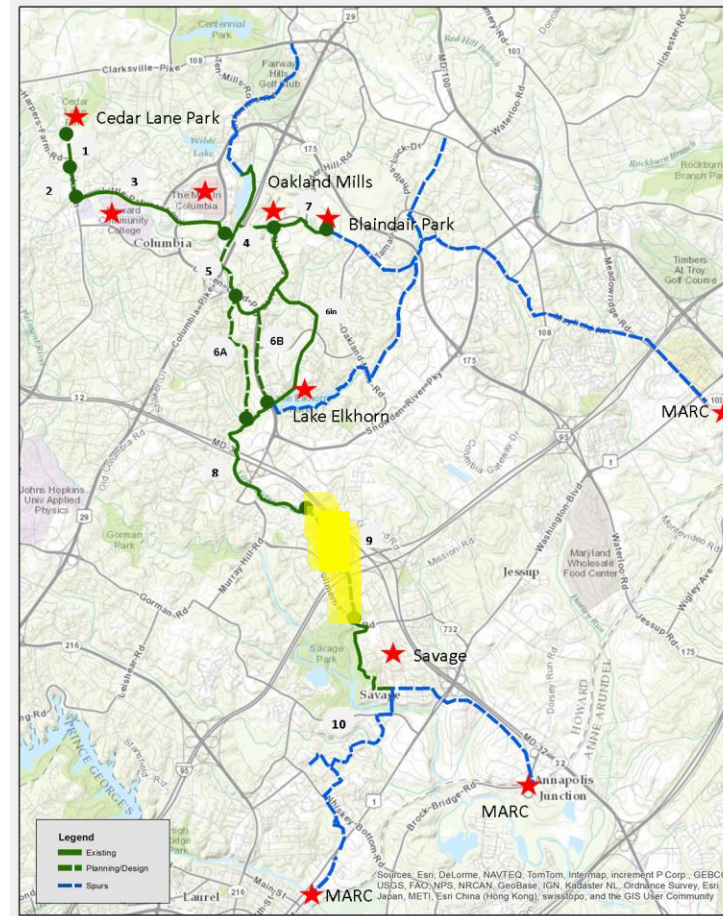
Introductions

- › Kris Singleton – DPW, Project Manager
- › Chris Eatough – Office of Transportation, Bicycle and Pedestrian Coordinator
- › Bruce Gartner – Office of Transportation, Administrator
- › Jason Thompson – Rec and Parks, Park Planner
- › Rick Adams – RK&K, Project Manager
- › Andrew Aguilar – RK&K, Senior Project Engineer



Patuxent Branch Trail - Overview

- › 4.6 Miles
- › Savage Park to Lake Elkhorn
- › Multi Use Trail
- › 8-10 foot Wide
- › Asphalt, 3.3 mi.
- › Packed Gravel, 1.3 mi.
- › Timber Boardwalks over Multiple Stream Crossings
- › Grade Separated Road Crossings – Broken Land Parkway, MD 32, Guilford Rd, I-95
- › One At-Grade Road Crossing – Vollmerhausen Rd
- › Primary Bike/Ped Link Between Columbia, Savage and Laurel



Patuxent Branch Trail – Existing Conditions

Vollmerhausen Road to Old Guilford Road

- › 1.3 Miles
- › Multi Use Trail
- › 8-10 foot Wide
- › Packed Gravel / Variable Surface
- › Guilford Pratt Truss Bridge
- › Timber Boardwalks over 2 Stream Crossings



Goals

- › Improve Accessibility
 - › Improve accessibility level to remainder of trail
 - › Current surface is gravel and soil. Potholes become puddles/ice patches after rain/snow/freeze.
 - › Upgrade to a consistent durable surface will benefit:
 - › People walking and jogging
 - › People with jogging or walking strollers
 - › People in wheelchairs, mobility devices and walking aids
 - › People on bicycles
- › Safety
- › Protect Park Resources
- › Minimize Environmental Impact
- › Minimize Maintenance
- › Enhance Sense of Place, Historic Context



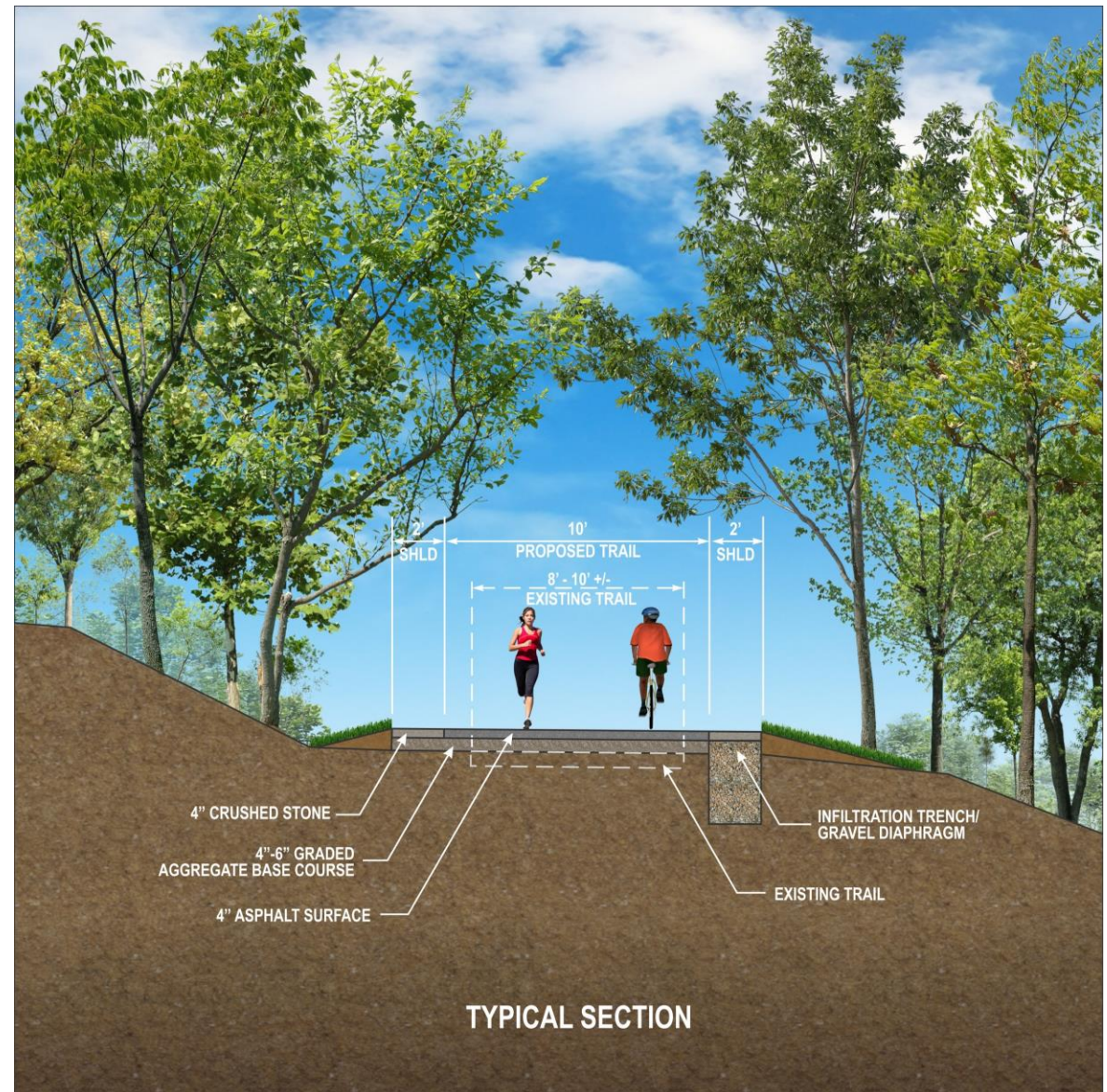
Project Needs

- › Narrow Width (8-10 ft)
- › Inconsistent Surface
- › Poor Drainage
- › Muddy Conditions
- › Vollmerhausen Road Crossing
- › Address Community Concerns



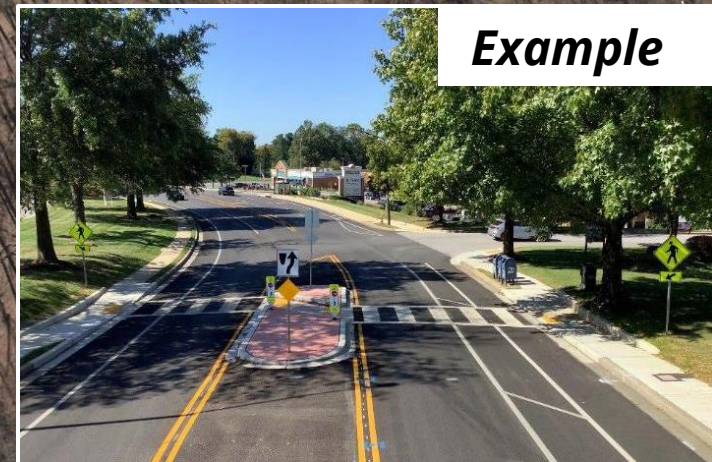
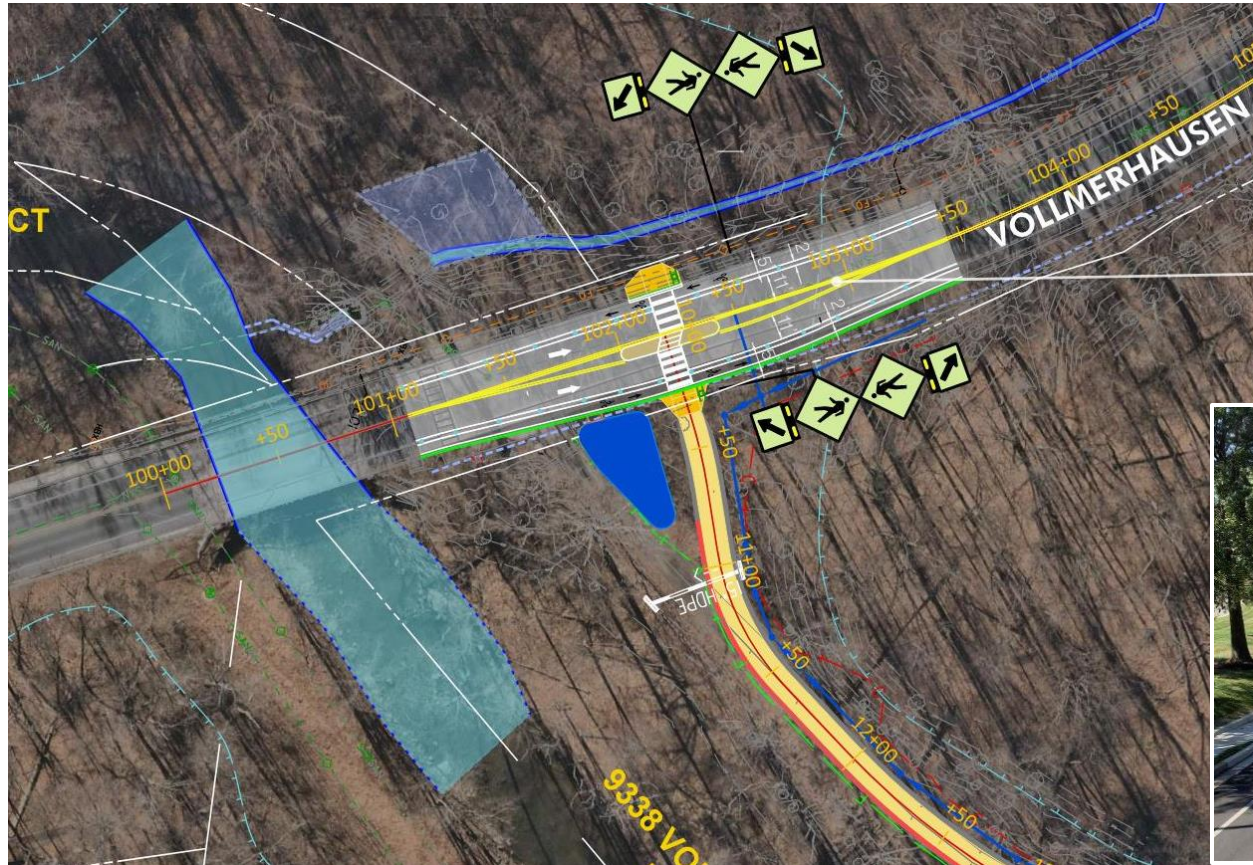
Original Improvements

- › Asphalt Pavement Surface
- › Flatten Grades < 5%
- › ADA Compliant
- › Stormwater Management
- › Drainage Conveyance - Eliminate Ponding on Trail
- › Replace Boardwalks
- › Wayfinding
- › Interpretive Signs



Improvements

- › Proposed Vollmerhausen Road Crossing



Community Feedback

- › Public Meeting and on site tabling at Running Event in January and February
- › Do not pave the trail
- › Reduce clearing and impacts to trees/forest
- › Reduce impacts to environmental resources
- › Reduce costs
- › Maintain a more natural stone surface
- › Accessibility to wheelchairs
- › This will make it easier to get to Savage
- › I whole heartedly support the plans for improving the drainage issues
- › "I live next to this trail and use it several times a week. I look forward to your planned improvements. They are long overdue."



Tree Impact Reductions

- › Jan 2022 Design – 300 Trees Impacted
 - › Proposed 10' Trail with 2 ft shoulders and Temporary River Crossing at Guilford Truss Bridge. All trees within close proximity to limit of grading were proposed to be removed.
- › April 2022 Design – 130 Trees Impacted
 - › Modified to 8'-10' variable width trail with 1-2 ft shoulders and drainage ditches. Eliminated Temporary Crossing of the River.
- › May 2022 Design – 50 Trees Impacted
 - › Developed No-Dig Detail to reduce grading and impacts to tree root systems.
 - › Utilizes Geotextile/Geogrid placed on existing ground to widen beyond existing trail
 - › Only trees within proposed trail/shoulder or within a significant amount of fill will be removed.

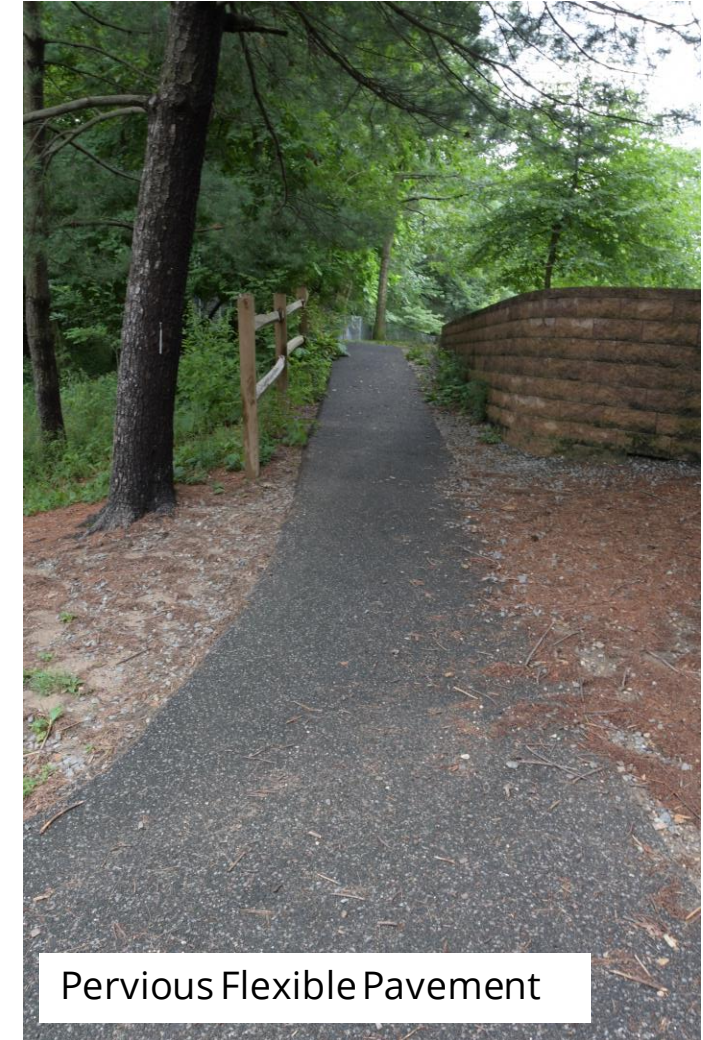


Alternative Surfaces Under Consideration

- › Pervious Asphalt
- › Pervious Flexible Pavements (crushed stone, crushed rubber, binder)
- › Bound Crushed Stone
- › Unbound Crushed Stone



Stone Surface



Pervious Flexible Pavement

Environmental Impact Reductions

- › Eliminate temporary crossing of Patuxent River at Old Guilford Rd
 - › All construction access will be made from Volmerhausen Road
- › Reduce width of trail and reduce grading
- › Utilize geotextiles to reduce excavation and impacts to root systems
- › Utilize permeable surfaces to provide water and oxygen to root systems.
- › Infiltrate water and reduce surface runoff
- › Maintain habitat of river and floodplain



Cost Reductions

- › Eliminate temporary river crossing
- › Eliminate retaining walls
- › Maintain current boardwalks; replace railings to increase clear width
- › Implement variable width trail
 - › 2,425 ft of 8' trail,
 - › 2,775' of 9' trail,
 - › 1,375' of 10' trail
- › Pervious surface will increase costs
- › Overall Reduction of Costs: \$500K – \$1M

Cost and Schedule

- Capital Project Number N3963
 - Short Term Priority in BikeHoward (Howard County Bicycle Master Plan)
 - Estimated Construction Cost: \$2.7M
 - Awarded Transportation Alternatives Program Grant For Design: \$1.1M
 - Pursuing Additional Grant Funding for Construction
-
- Preliminary (30%) Engineering: August 2022
 - Final (100%) Engineering: May 2023
 - Construction: To Be Determined Pending Grant Funding

Questions

Department of Public Works:

- › Web: www.howardcountymd.gov/public-works
- › Phone: (410) 313-6138
- › Contact: Kris Singleton, Project Manager
- › Email: ksingleton@howardcountymd.gov

Office of Transportation:

- › Web: www.howardcountymd.gov/transportation
- › Phone: (410) 313-0567
- › Contact: Chris Eatough, Bicycle and Pedestrian Coordinator
- › Email: ceatough@howardcountymd.gov

Department of Recreation and Parks:

- › Web: www.howardcountymd.gov/rap
- › Phone: (410) 313-4031
- › Contact: Jason Thompson, Park Planner
- › Email: jthompson@howardcountymd.gov

Project Web Site: <https://bikehoward.com/projects/surface-upgrade-for-existing-patuxent-branch-trail/>

Thank You for Your Feedback!